

Chapter 24K

EXHAUST SYSTEM, DISMANTLING

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THIS CHAPTER, which is applicable to both the Ghost 48 Mk. 1 and the Ghost 48 Mk. 2, contains instructions for dismantling the exhaust system, after it has been removed from the engine in accordance with the instructions given in chapter 23. The general information contained in chapter 22 should be referred to as necessary.

Due to the heat to which the exhaust cone assembly is subjected during running, all nuts, bolts, and studs should be soaked in penetrating oil for a minimum period of two hours before dismantling and rotated slowly during removal, otherwise they are liable to fracture and shear. Should this occur, it may be difficult to remove the sheared portion. If mod. 1081 and 1218 have been embodied, the threads of most of the bolts and screws in the exhaust system will be copper plated, thus reducing their tendency to seize. Before commencing to dismantle the exhaust cone assembly, mount it on stand T.75700, Fig. 1, and attach the large ring to the rear end of the exhaust cone by wire.



Fig. 1. Exhaust cone assembly mounted on stand T.75700. The large ring, which is attached by wire to the rear end of the outer cone, will provide support at this point during dismantling and re-assembling.

PROPELLING NOZZLE

1. Remove the thirty-six plain $\frac{1}{4}$ in. B.S.F. bolts and nuts, also the four lockwire tabs, which secure the propelling nozzle assembly to the

outer cone, Fig. 2; remove the drain pipe and adapter from the propelling nozzle.

2. If it is necessary to dismantle the adjustable type of propeller nozzle, remove the three 2 B.A. plain nuts, spring washers and counter-sunk socket screws which secure each trimming strip; tie the strips together and secure them to the propelling nozzle.

FRONT BAFFLE

Unlock and remove the sixteen $\frac{1}{4}$ in. B.S.F. screws which secure the front baffle to the inner cone; remove the tab washers and lift off the baffle.

SUPPORT TUBES AND INNER CONE

Remove the four cap-nuts and washers from the end of the two rear support tubes and using a soft drift, drive out the four support

tubes. As the second of the two front support tubes is removed, the air tube which is in the centre of the inner cone will be released and must be removed. Lift out the inner cone.

FAIRINGS AND OUTER CONE

There are two pairs of fairing assemblies and the fairings in each pair are different. It is advisable, therefore, to paint a number on all four fairings and to paint the corresponding number on the inner and outer cones adjacent to the position of the respective fairings.

1. Cut the locking wire and remove the four set-bolts and washers which secure each of the fairings; take out the fairings.
2. Unscrew the blanking nuts from the unused pressure point and thermocouple connections.
3. Remove the outer cone from the stand.

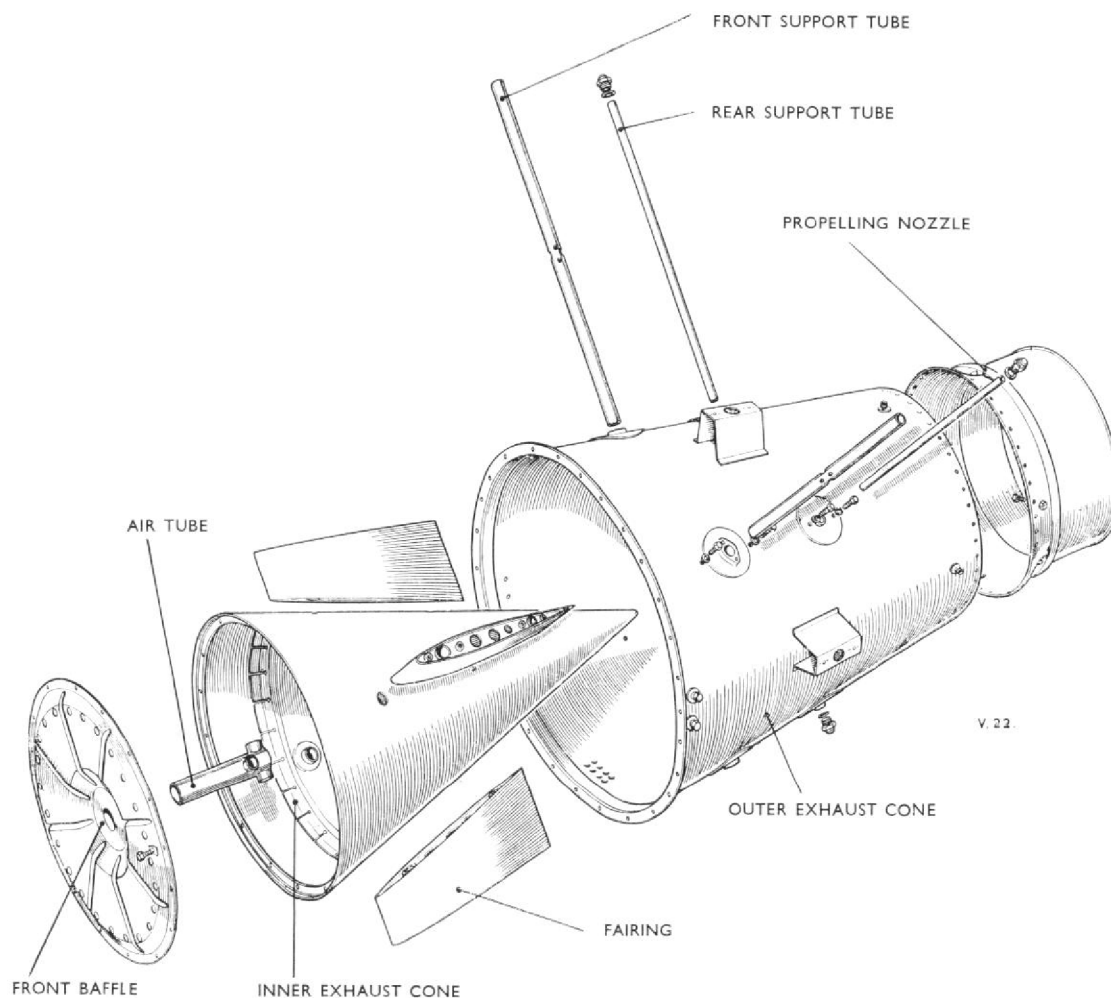


Fig. 2. Exhaust cone assembly.

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